



MIRAFI H₂RI COUNTY ROAD APPLICATIONS

FEBRUARY 3, 2026

Why Mirafi H2Ri?

- Poor Subgrade/wet conditions on existing roads
- Need to reduce peat/muck excavation on local jobs to keep costs down
- Moisture Wicking keeps aggregate base from getting saturated
- History of different methods with various levels of success.
 - What do we have to lose?



Mirafi H2Ri

- Hybrid Reinforcement + separation
- Designed for weak subgrades
- Improves load distribution
- Reduces aggregate thickness (keeps weight off unstable soils)



32ND ST - SALEM TWP

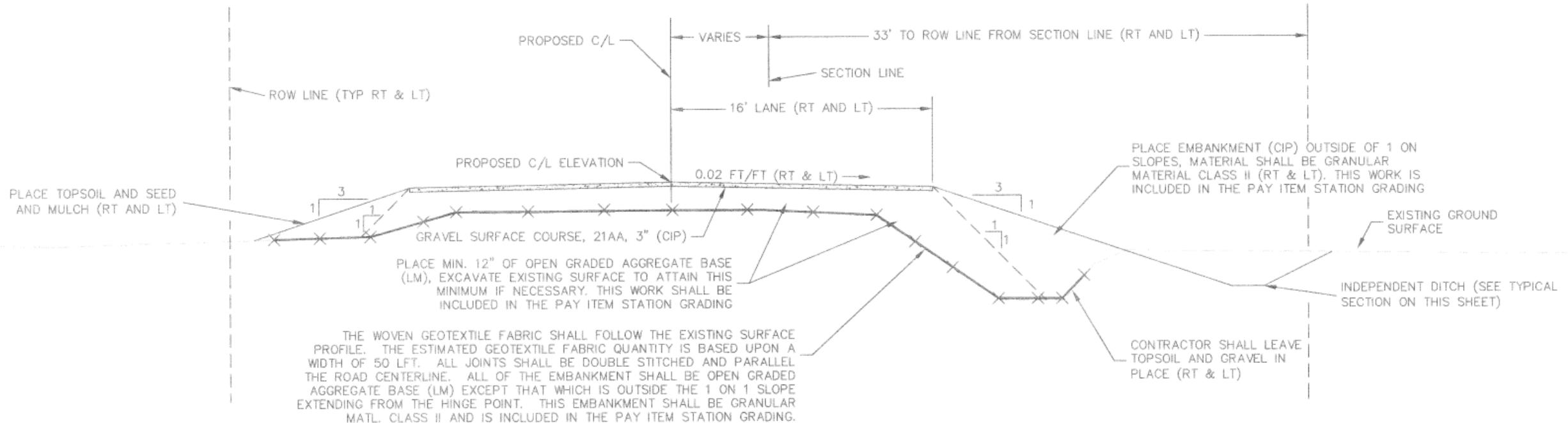
HISTORY

- ROAD WAS RECONSTRUCTED IN 1997
- PAVED IN 1998 WITH 2" OF HMA
- PEAT SECTION WAS DEEP AND WATER TABLE JUST BELOW SUBGRADE
- GEOTEXTILE WRAP WAS USED ON THE EXISTING GRAVEL ROAD (BURRITO WRAP)
- OPEN GRADED AGGREGATE WAS USED AS EMBANKMENT MATERIAL INSIDE THE WRAP
- FOLLOWED BY 3" OF 21AA LIMESTONE
- CRUSHED AND SHAPED IN 2010 WITH 3" OF HMA

EXISTING CONDITION

- SOIL BORINGS SHOWED 5" TO 12" OF HMA
- ROAD HAD SETTLED 18" FROM AS CONSTRUCTED PROFILE IN THE WORST SECTION
- AREA WAS PATCHED EXTENSIVELY





TYPICAL GEOTEXTILE SECTION
39+50 TO 50+00

32ND ST - SALEM TWP

RECONSTRUCTED WITH MIRAFI – 2023

- REMOVED 1200' OF PAVEMENT
- PLACED 6" TO 18" SAND ON TOP OF EXISTING GEOTEXTILE STABILIZATION
- RE-ESTABLISHED VERTICAL PROFILE
- PLACED 1200' OF MIRAFI H2Ri
- PLACED 6" OF 22A SLAG
- PAVED BASE/LEVELING COURSE WITH 1 $\frac{1}{2}$ " OF HMA
- PLACED ANOTHER 1 $\frac{1}{2}$ " ON TOP COURSE AS PART OF OUR LOCAL PAVING







LOCATION 2 – 124TH AVE



LOCATION 3 – 138TH AVE



LOCATION 4 – 146TH AVE



SO HOW IS IT PERFORMING?







THANK YOU

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