



# MIRAFI H<sub>2</sub>RI COUNTY ROAD APPLICATIONS

FEBRUARY 3, 2026

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# Why Mirafi H2Ri?

- Poor Subgrade/wet conditions on existing roads
- Need to reduce peat/muck excavation on local jobs to keep costs down
- Moisture Wicking keeps aggregate base from getting saturated
- History of different methods with various levels of success.
  - What do we have to lose?





# Mirafi H2Ri

- Hybrid Reinforcement + separation
- Designed for weak subgrades
- Improves load distribution
- Reduces aggregate thickness (keeps weight off unstable soils)





# 32ND ST - SALEM TWP

## HISTORY

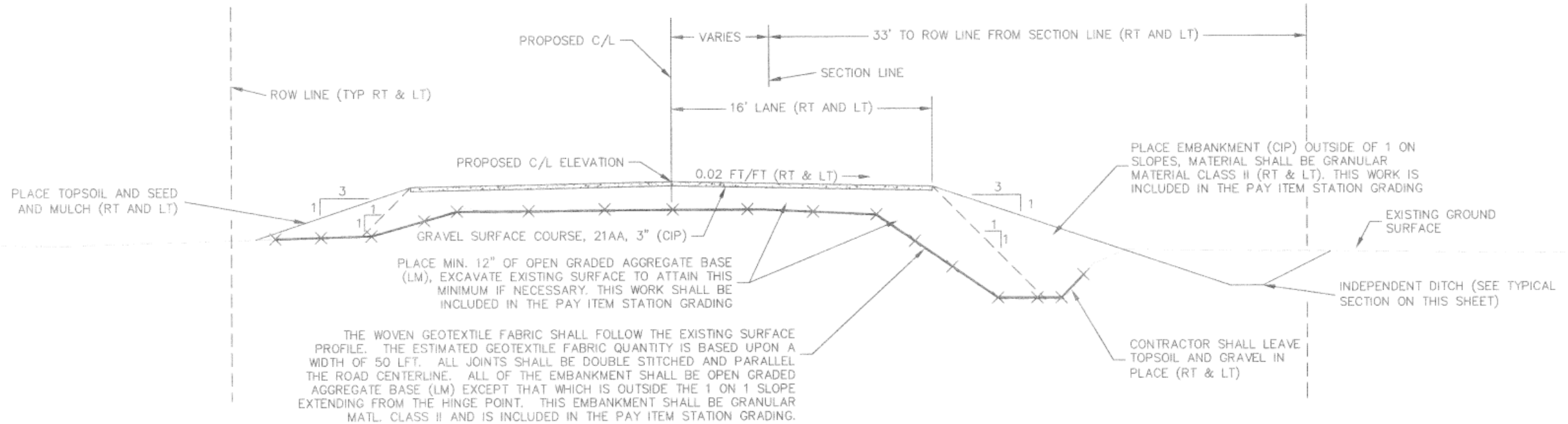
- ROAD WAS RECONSTRUCTED IN 1997
- PAVED IN 1998 WITH 2" OF HMA
- PEAT SECTION WAS DEEP AND WATER TABLE JUST BELOW SUBGRADE
- GEOTEXTILE WRAP WAS USED ON THE EXISTING GRAVEL ROAD (BURRITO WRAP)
- OPEN GRADED AGGREGATE WAS USED AS EMBANKMENT MATERIAL INSIDE THE WRAP
- FOLLOWED BY 3" OF 21AA LIMESTONE
- CRUSHED AND SHAPED IN 2010 WITH 3" OF HMA

## EXISTING CONDITION

- SOIL BORINGS SHOWED 5" TO 12" OF HMA
- ROAD HAD SETTLED 18" FROM AS CONSTRUCTED PROFILE IN THE WORST SECTION
- AREA WAS PATCHED EXTENSIVELY







**TYPICAL GEOTEXTILE SECTION  
39+50 TO 50+00**



# 32ND ST - SALEM TWP

## RECONSTRUCTED WITH MIRAFI – 2023

- REMOVED 1200' OF PAVEMENT
- PLACED 6" TO 18" SAND ON TOP OF EXISTING GEOTEXTILE STABLIZATION
- RE-ESTABLISHED VERTICAL PROFILE
- PLACED 1200' OF MIRAFI H2Ri
- PLACED 6" OF 22A SLAG
- PAVED BASE/LEVELING COURSE WITH 1 ½ " OF HMA
- PLACED ANOTHER 1 ½ " ON TOP COURSE AS PART OF OUR LOCAL PAVING













## LOCATION 2 – 124TH AVE





## LOCATION 3 – 138TH AVE





## LOCATION 4 – 146TH AVE





## SO HOW IS IT PERFORMING?











# THANK YOU

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